

PLANNING ENVIRONMENT ACT 1987
SURF COAST PLANNING SCHEME
PLAN
THE DEVELOPMENT/USE IS DEEMED SATISFACTORY
PURSUANT TO 43.04.09 OF THE SCHEME
(DPO9)
Signed *[Signature]*
for and on behalf of the Council of the Surf Coast Shire
Date: 24/1/2017 Sheet No. ...I. of 18. sheets.



DEVELOPMENT PLAN

DEVELOPMENT PLAN OVERLAY SCHEDULE 9


WINCHELSEA COMMERCIAL 2 ZONE AREA

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1 INTRODUCTION

This Development Plan is a plan to guide the form and conditions of future use and development within the Winchelsea Development Plan Overlay Schedule 9 area, which is a designated future Commercial Area included in the Commercial 2 Zone.

Amendment C65 re-zoned the land that this subject to this Development Plan (Subject Site) from Farming Zone to at that point in time Business 4 Zone, later translated into the Commercial 2 Zone. The Subject Site was identified in the Winchelsea Structure Plan 2021 as a site where the Business 4 Zone should be applied to accommodate "restricted retail" type uses.

Development Plan Overlay Schedule 9 is the planning instrument within the Surf Coast Planning Scheme that gives statutory effect to the strategic intent of the Winchelsea Structure Plan 2021 and Amendment C65. This Development Plan has been prepared to satisfy the requirements of Development Plan Overlay Schedule 9.

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2 SITE AND CONTEXT

The Subject Site is located to the south west of the Winchelsea township, and abuts the Princes Highway and Anderson Street (Winchelsea Deans Marsh Road).

The subject site is approximately 6.6ha in area, and comprises the following properties and titles:

Lot R1 PS718344 (ROAD parcel) – approx. 0.0135ha
2931 Princes Highway (Lot 2 PS718344, part Lot 1 PS718344) – approx. 0.8ha
2935 Princes Highway (Lot 1 TP201131) – approx. 0.15ha
2945 Princes Highway (Lot 1 LP111822) – approx. 1.1ha
(Part) 2955 Princes Highway (Lot 1 TP170902) – approx. 1.6ha
(Part) 2995 Princes Highway (Por. Q PARISH OF MIRNEE) - approx.3.0ha

As detailed in Figure 1, the majority of the Subject Site is cleared farmland however part of the Subject Site contains an existing farm dwelling and associated sheds, and part of the Subject Site is currently used for light industrial purposes (bulk haulage, concrete tank supply).

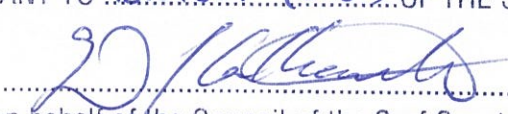
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Figure 1: Subject Site



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The Existing and Strategic Land Use and Planning Context

The Subject Site is a designated "Commercial Area" on the Winchelsea Framework Map (Map 1 to Clause 21.09). This designation stems from the lands' strategic identification for potential re-zoning in the Winchelsea Structure Plan 2021, and the subsequent re-zoning of the Subject Site to Business 4 Zone via Amendment C65 (this land use zone was subsequently incorporated into the new Commercial 2 Zone). Amendment C65 also applied:

- Design and Development Overlay 17 (whole of Subject Site)
- Development Plan Overlay 9 (whole of Subject Site)
- Environmental Audit Overlay (part of Subject Site – see Figure 2)




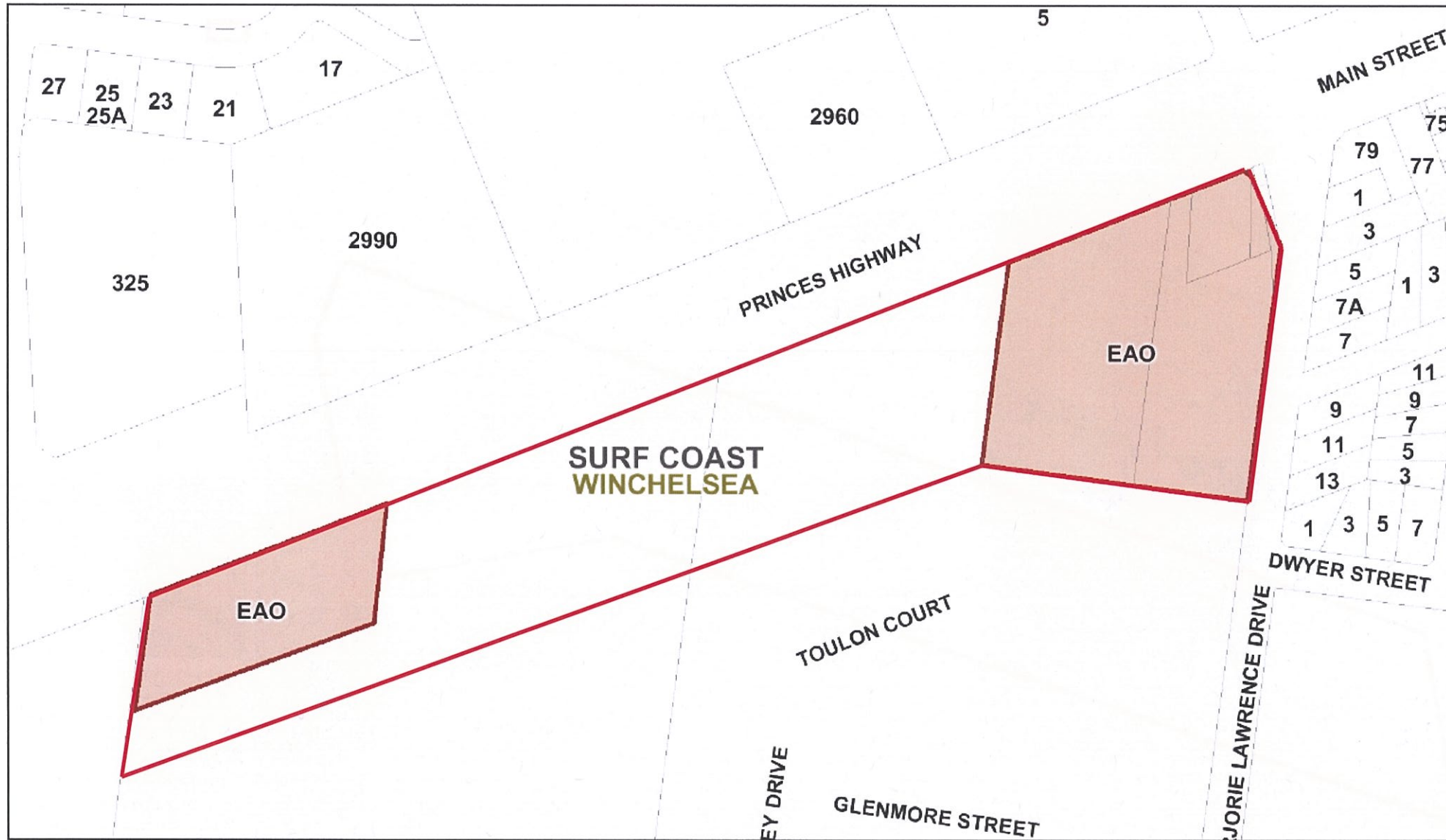
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Figure 2: Extent of EAO



Relevant Flora and Fauna and Archaeological Constraints

The Subject Site itself does not contain any areas of native vegetation (refer Reference Document 1 & Figure 3). The Princes Highway road reserve immediately north of the Subject Site contains two patches of native vegetation. These two patches of native vegetation have been identified for removal by VicRoads in order to facilitate the Princes Highway Duplication Project (refer Reference Document 2). VicRoads is therefore responsible for the removal and offsetting of this native vegetation.

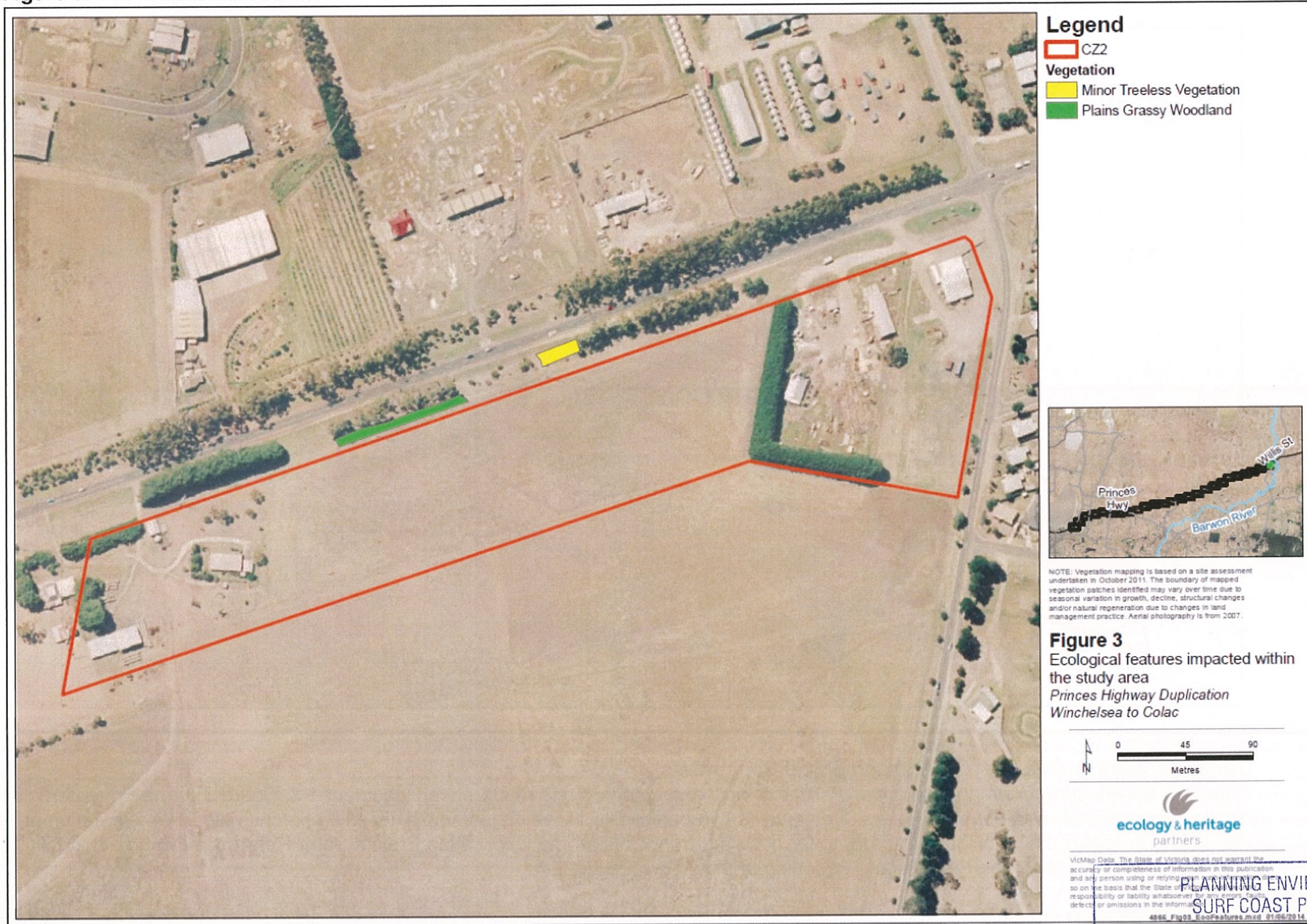
The Subject Site is not a defined area of Aboriginal Cultural Heritage Sensitivity and the proposed activity (i.e. approval of a Development Plan) is not a high impact activity.

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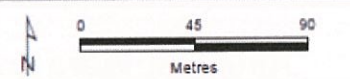
Figure 3: Environmental Assets



- Legend**
- CZ2
 - Vegetation**
 - Minor Treeless Vegetation
 - Plains Grassy Woodland

NOTE: Vegetation mapping is based on a site assessment undertaken in October 2011. The boundary of mapped vegetation patches identified may vary over time due to seasonal variation in growth, decline, structural changes and/or natural regeneration due to changes in land management practice. Aerial photography is from 2007.

Figure 3
Ecological features impacted within the study area
*Princes Highway Duplication
Winchelsea to Colac*



VictMap Data. The State of Victoria does not warrant the accuracy or completeness of information in this publication and any person using or relying on this information does so on the basis that the State of Victoria accepts no responsibility or liability whatsoever for any errors, omissions, defects or omissions in the information.

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Urban Services

Servicing requirements are detailed the attached urban servicing assessment (refer Reference Document 3).

Sewer

The entire C2Z land is capable of being serviced by connecting to the sewerage network that has been either design and/or constructed to service the GRZ1 land to the immediate south. This will require:

- All C2Z land excluding 2995 Princes Highway to connect to the designed / constructed sewer that runs along the northern boundary of the G1Z land to the immediate south; and
- The provision of a 3m wide easement (to accommodate both sewer and drainage) along the southern boundary of the C2Z land on 2995 Princes Highway

Water

Reticulated water can be provided to the Subject Site by extending a 150mm main along the Princes Highway from the existing 150mm PVC main at the corner of Anderson Road.

Drainage

The section of the C2Z land (encompassing all properties except 2995 Princes Highway) is serviced by the drainage detention basin that has been constructed as part of the first stages of development of the GRZ1 land to the immediate south.

A new drainage detention basin will need to be constructed to service the 3ha section of C2Z land located on 2995 Princes Highway. Because this drainage detention basin will only provide detention for 2995 Princes Highway, that property will be 100% responsible for all costs associated with this basin.

The C2Z land located on 2995 Princes Highway will be connected to this future drainage detention basin via a 3m wide easement (to accommodate both sewer and drainage) running along its' southern boundary.


A 2m wide drainage easement will be required to be provided along the southern boundary of the C2Z land on 2955 Princes Highway, to enable the future drainage detention basin on the C2Z land on 2995 Princes Highway to connect to the existing drainage network.

Site Topography

The surface gradient of the Subject Site is relatively flat, with an overall slope downwards towards the east-south-east.

Views and Vistas

Due to its' relatively flat topography the Subject Site is currently reasonably visible from the surrounding public realm, including from the Princes Highway. The visibility of the site from the Princes Highway will increase once the duplication project is completed, and the areas of roadside vegetation within the highway reservation abutting the Subject Site are removed to facilitate that project. At present there are no particularly noteworthy or protected views of vistas either to or from the Subject Site (refer Reference Document 4).

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3 ANTICIPATED LAND USES

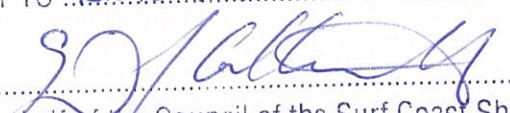
The Subject Site is zoned C2Z. A purpose of this zone is to encourage commercial areas for offices, appropriate manufacturing and industries, bulky goods retailing, other retail uses, and associated business and commercial services.

Pursuant to the requirements of DPO9, an economic analysis has been undertaken by Essential Economics into the need for the Subject Site to provide a range of larger lot sizes, which are capable of accommodating commercial uses requiring a greater land area such as a service station, bulky goods retailing, caravan or motor vehicle sales (refer Reference Document 5). That analysis found that the following key factors will influence the nature of future land uses at the Subject Site:

- Land size of approximately 6.5ha is sufficiently large to support a variety of land uses requiring large sites;
- Existing land use zone (C2Z) and planning controls (DPO9);
- Exposure and access to Princes Highway will suit businesses that require exposure to high volumes of vehicular traffic;
- The thin configuration of the site will restrict the type of land uses that can be accommodated. For instance, an internalized homemaker centre could not be accommodated on the site, but a string of showroom uses could;
- Location within a broader agricultural region;
- Location on the Colac-bound (west-bound) side of Princes Highway is particularly important for the prospects of a service station, having regard for the location of an existing service station on the Geelong-bound (east-bound) side of the Princes Highway;
- Access to a significant number of trucks that will pass the subject land;
- Location within a relatively small township that does not serve a large residential population will limit opportunities for 'population' serving businesses such as some forms of retailing; and
- Location opposite an existing industrial estate presents an opportunity to contribute to an employment precinct on the western edge of town.

Based upon these key influencing factors, the economic analysis concluded that the following land uses are considered to be 'potential' development opportunities for the Subject Site:

- Service station and associated uses (e.g. fast-food outlet, car wash, mechanic)
- Motor Vehicle Sales
- Caravan Sales
- Rural Machinery and Supplies

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4 ANTICIPATED LOT SIZE MIX

The economic analysis prepared by Essential Economics confirms that the mix of land uses that can reasonably be expected to establish upon the Subject Site typically require the following range of lot sizes:

Service station (without ancillary uses):	1,500m ² – 2,000m ²
Service Station (with ancillary uses):	2,500m ² – 3,000m ²
Motor Vehicle Sales:	1,000m ² – 4,000m ²
Caravan Sales:	1,000m ² – 4,000m ²
Rural Machinery & Supplies (typical)	2,000m ² – 4,000m ²
Rural Machinery & Supplies (large)	1ha

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5 DEVELOPMENT PLAN

A specific lot layout has not been defined for the Subject Site. This is a deliberate action undertaken in the absence of certainty regarding the intended mix of use and development across the Subject Site and the sequence of development. While the type of land uses that could reasonably be expected to establish upon the Subject Site and their typical land area requirements are known, the specific distribution of such land uses across the Subject Site is not.

In such a circumstance, a defined lot layout has the potential to frustrate otherwise acceptable land use and development proposals from being established. Accordingly, the Development Plan provides a framework and guidance to enable land use and development proposals to be considered and (if appropriate) facilitated in accordance with the requirements of the Zone and Overlay controls, without prejudicing the overall use and development of the Subject Site.

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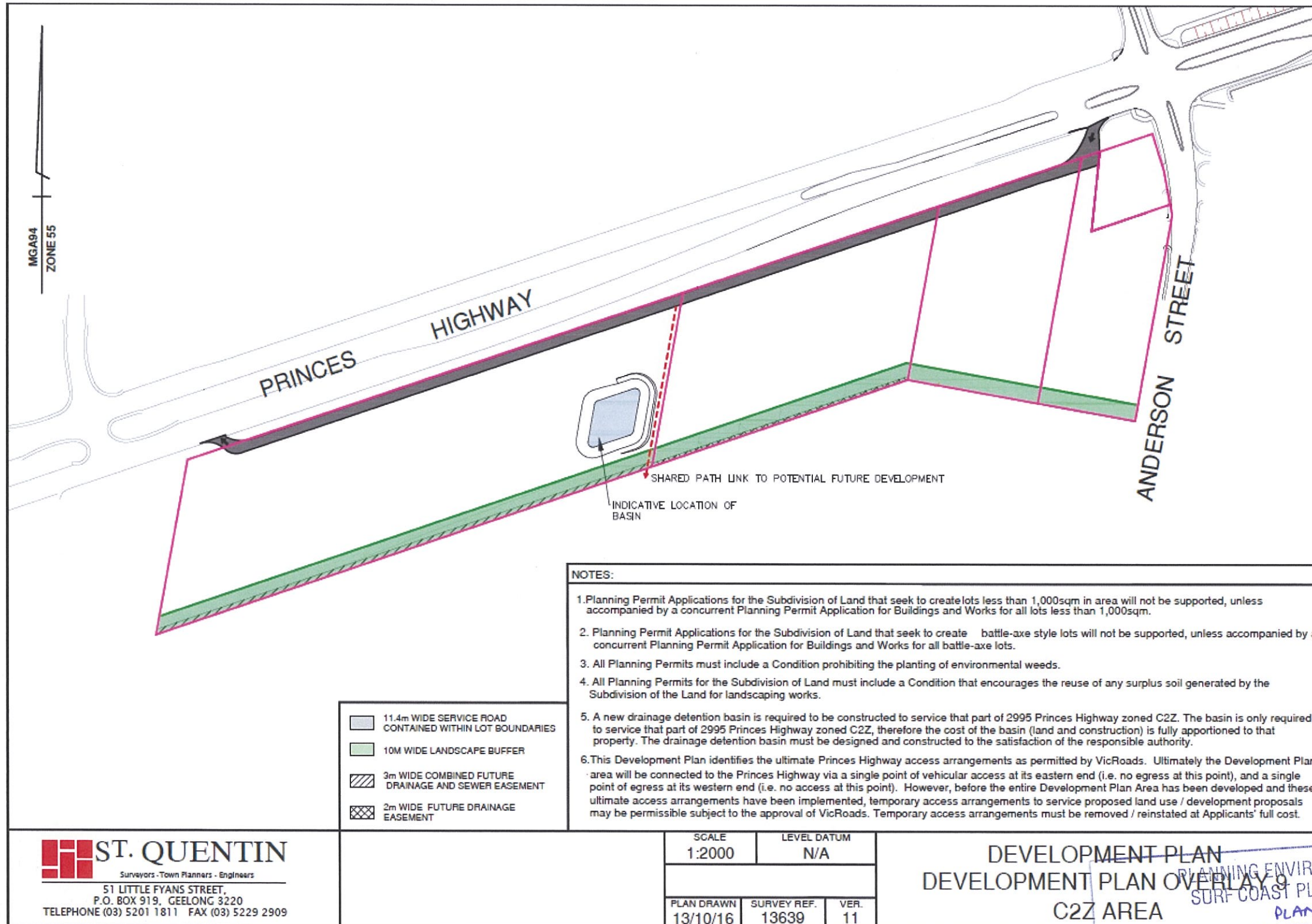
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Figure 4: Development Plan – DPO9 – Winchelsea C2Z Area




- NOTES:**
1. Planning Permit Applications for the Subdivision of Land that seek to create lots less than 1,000sqm in area will not be supported, unless accompanied by a concurrent Planning Permit Application for Buildings and Works for all lots less than 1,000sqm.
 2. Planning Permit Applications for the Subdivision of Land that seek to create battle-axe style lots will not be supported, unless accompanied by a concurrent Planning Permit Application for Buildings and Works for all battle-axe lots.
 3. All Planning Permits must include a Condition prohibiting the planting of environmental weeds.
 4. All Planning Permits for the Subdivision of Land must include a Condition that encourages the reuse of any surplus soil generated by the Subdivision of the Land for landscaping works.
 5. A new drainage detention basin is required to be constructed to service that part of 2995 Princes Highway zoned C2Z. The basin is only required to service that part of 2995 Princes Highway zoned C2Z, therefore the cost of the basin (land and construction) is fully apportioned to that property. The drainage detention basin must be designed and constructed to the satisfaction of the responsible authority.
 6. This Development Plan identifies the ultimate Princes Highway access arrangements as permitted by VicRoads. Ultimately the Development Plan area will be connected to the Princes Highway via a single point of vehicular access at its eastern end (i.e. no egress at this point), and a single point of egress at its western end (i.e. no access at this point). However, before the entire Development Plan Area has been developed and these ultimate access arrangements have been implemented, temporary access arrangements to service proposed land use / development proposals may be permissible subject to the approval of VicRoads. Temporary access arrangements must be removed / reinstated at Applicants' full cost.

-  11.4m WIDE SERVICE ROAD CONTAINED WITHIN LOT BOUNDARIES
-  10M WIDE LANDSCAPE BUFFER
-  3m WIDE COMBINED FUTURE DRAINAGE AND SEWER EASEMENT
-  2m WIDE FUTURE DRAINAGE EASEMENT

ST. QUENTIN
 Surveyors - Town Planners - Engineers
 51 LITTLE FYANS STREET,
 P.O. BOX 919, GEELONG 3220
 TELEPHONE (03) 5201 1811 FAX (03) 5229 2909

SCALE 1:2000	LEVEL DATUM N/A
PLAN DRAWN 13/10/16	SURVEY REF. 13639
VER. 11	

**DEVELOPMENT PLAN
 DEVELOPMENT PLAN OVERLAY 9
 C2Z AREA**

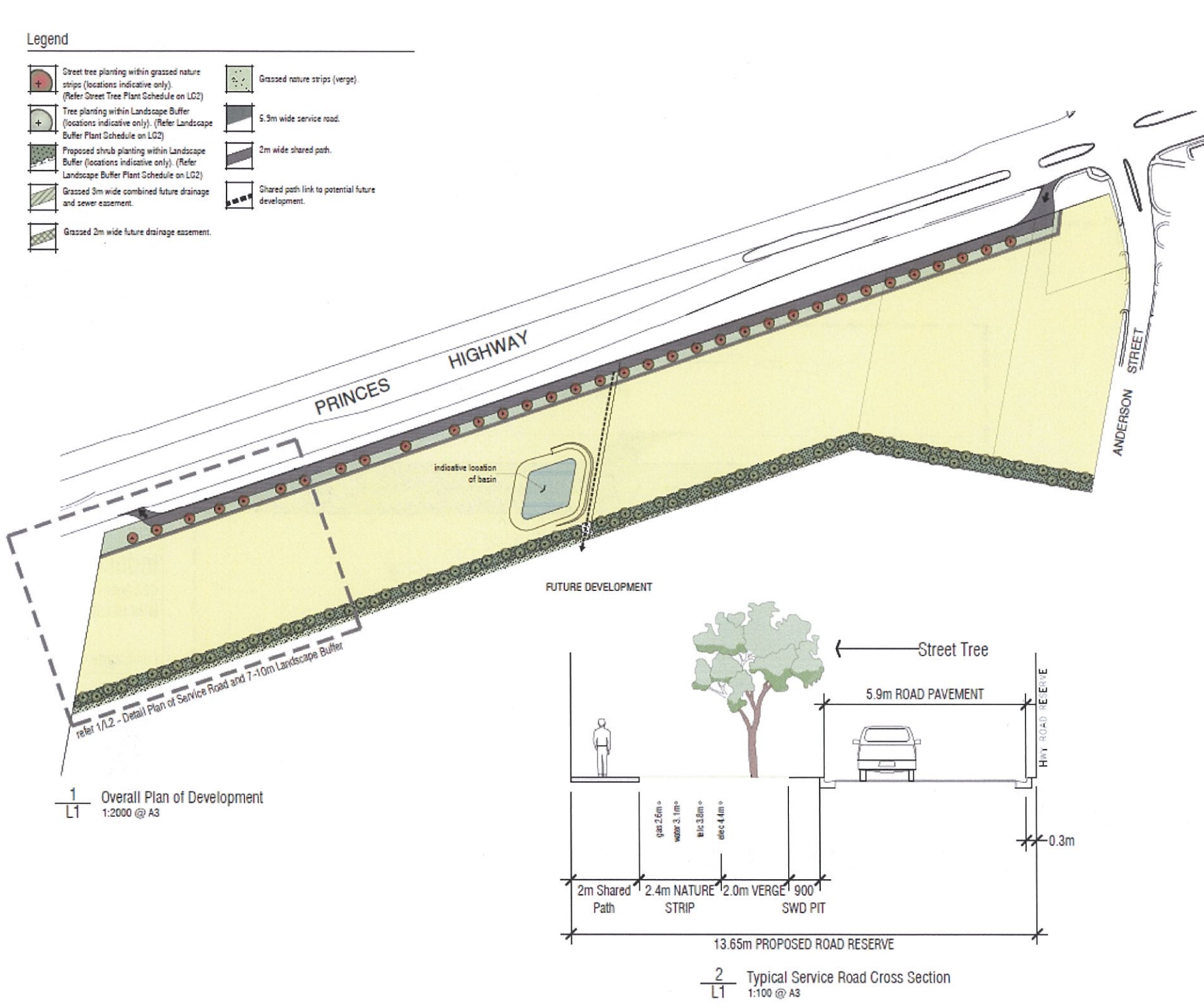
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6 LANDSCAPE CONCEPT PLANS



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Figure 5: Landscape Concept Plan – Overview Plan



1 Overall Plan of Development
L1 1:2000 @ A3

2 Typical Service Road Cross Section
L1 1:100 @ A3

No.	Issue	Date
01	Planning Permit Issue	18.05.2016
02	Development Plan Re-issue	22.08.2016
03	Development Plan Re-issue	17.10.2016
04	Development Plan Re-issue	16.12.2016
05	Development Plan Re-issue	16.01.2017

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Industrial Estate
C2Z Area - Anderson Street
WINCHELSEA
Landscape Concept Plan

MEXTED
LANDSCAPE ARCHITECTS
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project no: 2929
drawing no: LC1.05
sheet no: 1 of 2
designed by: GM
drawn by: EJ
date: 16.01.2017
scale: 1:2000 @ A3

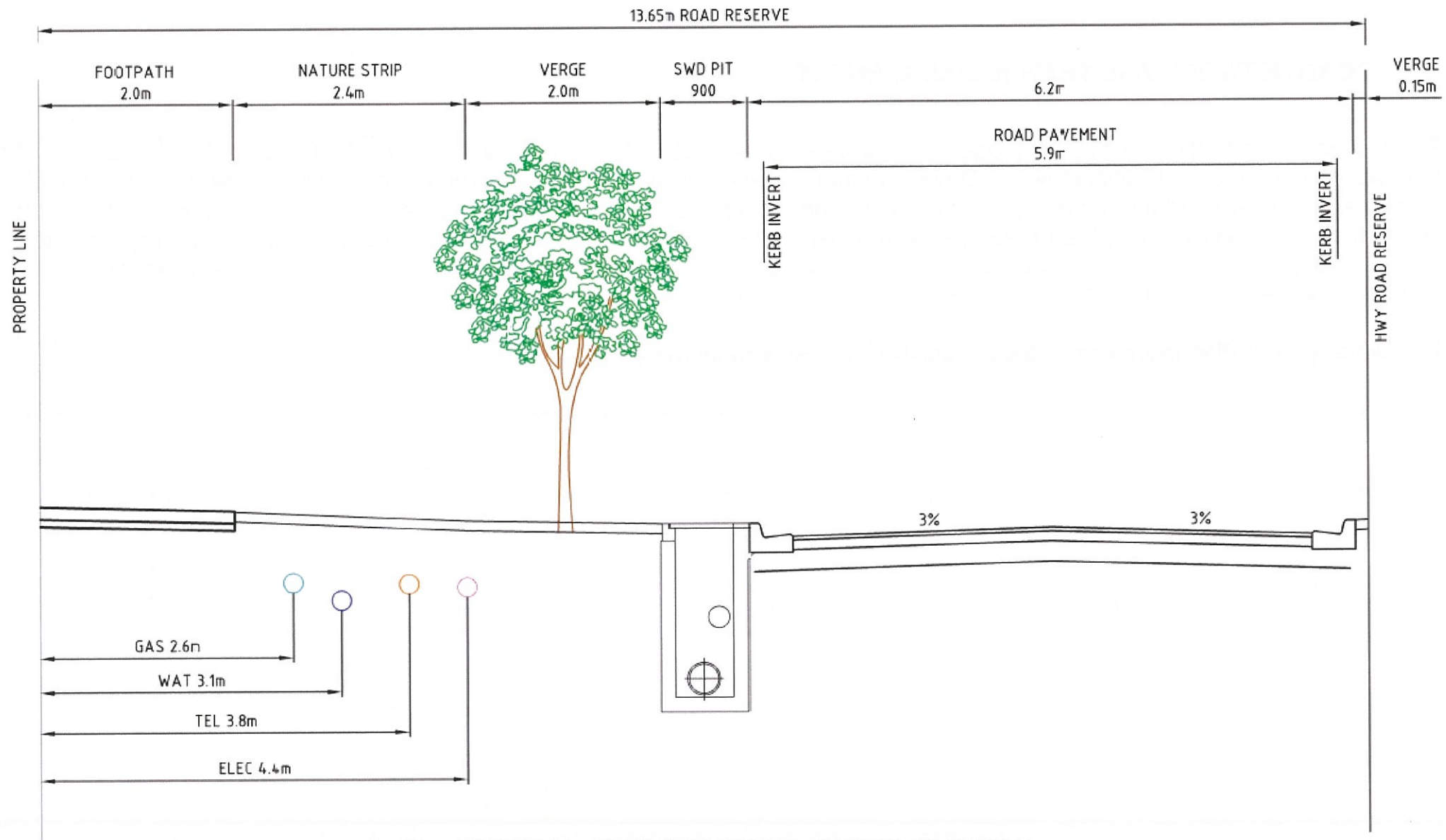
7 ROAD NETWORK AND TRAFFIC MANAGEMENT

Road network and traffic management requirements were the subject of detailed discussions with VicRoads in 2014 and 2016. Those discussions confirmed that ultimately, the Development Plan Area will be connected to the Princes Highway via a single point of vehicular access at its eastern end (i.e. no egress at this point), and a single point of egress at its western end (i.e. no access at this point). However, before the entire Development Plan Area has been developed and these ultimate access arrangements have been implemented, temporary access arrangements to service proposed land use / development proposals may be permissible subject to the approval of VicRoads. Temporary access arrangements must be removed/reinstated at the Applicants' full cost.

The Development Plan incorporates these specified access arrangements.

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Figure 7: Service Road Cross Section



13.65m TYPICAL SERVICE ROAD CROSS SECTION

NOT TO SCALE

NOTE: WIDTHS AND OFFSETS TO BE CONFIRMED DURING DESIGN PHASE.

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